

**Volusia County
MPO**

June 2009



Osceola Elementary School

Bicycle and Pedestrian School Safety Review Study

Phase 3C



Volusia County MPO



Transportation
Planning

www.VolusiaCountyMPO.com

Hoke
Design, Inc.

School Summary

Osceola Elementary School is located within the City of Ormond Beach. Of the 458 students attending this school, 105 live within the designated walk zone area. During the 2008/2009 school year, approximately 15 of these students walked or rode bicycles to school. No students wore helmets to school.

At the time of this Study, there were no crossing guard locations serving Osceola Elementary School students. There were no reported pedestrian crashes during estimated school travel times involving an elementary school age child within the walk zone from 2005-2007.

A sidewalk on the east side of Halifax drive from S.R. 40 (Granada Boulevard) to Amsden Road is recommended as the Priority Project for this school.



Osceola Elementary School – Summary Sheet

School Address: 100 Osceola Avenue
Ormond Beach, Florida 32176

School Principal: Earl Johnson
Phone: 386-258-4669
Email: eajohnso@volusia.k12.fl.us

Volusia County School District Representative: Saralee Morrissey, AICP
Phone: 386-947-8786, ext. 50772
Email: smorriss@volusia.k12.fl.us

Volusia County Public Works Traffic Engineering: Jon Cheney, P.E., Director
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Email: jcheney@co.volusia.fl.us

City Representative: Ric Goss, Planning Director
City of Ormond Beach
Phone: 386-676-3343
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Volusia County MPO: Stephan Harris, Bicycle and Pedestrian Coordinator
Phone: 386-226-0422 ext. 34
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Consultant Representative: Ginger Hoke, RLA, Hoke Design, Inc.
Phone: 407-923-6027
Email: ginger@hokedesign.com

Site Visit Observation Date: December 3, 2008 (weather: clear and cold)

Meeting Attendees:

- Earl Johnson
- Ric Goss
- Stephan Harris
- Ginger Hoke

Number of Enrolled Students (2008/2009 data): 458

Estimated Number of Students Living in Walk Zone: 105

Number of School Buses: 3

Number of Aftercare Buses/Vans: 4

Estimated Number of Walkers/Bicycle Riders: 15

Findings and Recommendations

Finding: The Visitor Parking area does not have pavement markings and the accessible parking spaces are not well defined.

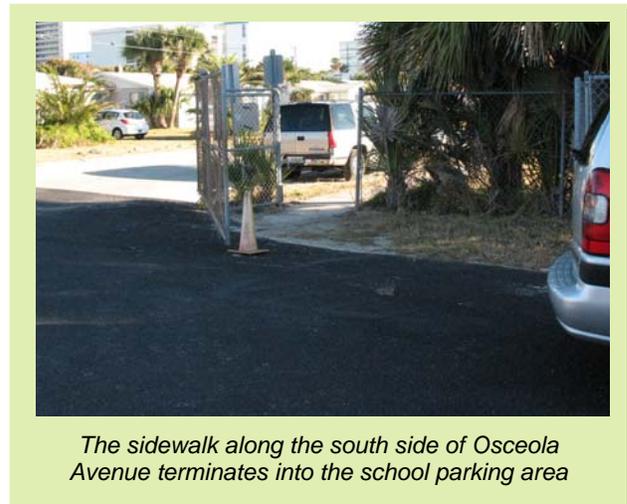
Recommendation: Provide pavement markings and include striping details to meet current ADA regulations (see Appendix H). Accessible parking should be located as close to the main building entrance as possible.



Finding: There are no sidewalks connecting the sidewalk along the south side of Osceola Avenue to the school entrance.

Recommendation: Provide a sidewalk connecting the sidewalk along the south side of Osceola Avenue to the school building.

Alternatively, when striping the parking lot, include a painted pedestrian zone connecting the sidewalk along Osceola Avenue to the main building entrance. The pedestrian area should be in front of any parked cars and separated from vehicles with bollards or curb stops.



Finding: The sidewalk along the student drop-off and pick-up area does not connect to the off-campus sidewalk along Coquina Drive. A sidewalk does connect the parking area to the school campus.

Recommendation: Construct the missing sidewalk connection to the Coquina Drive sidewalk.



Finding: Students who bicycle from north of Osceola elementary School via Coquina Drive do not have a direct sidewalk connection to the bicycle rack.

The existing bicycle rack concrete area is narrow with no paved access.

Recommendations: If possible, add a second bicycle rack near the Coquina Drive entrance, possibly near the park facilities.

If funds allow, expand the concrete area at the existing bicycle rack to provide paved access.



The concrete area for the bicycle rack is narrow

Finding: Motorists park along the sidewalk on the north side of Coquina Drive across from the student drop-off and pick-up area.

Recommendation: The City of Ormond Beach should add "no stopping or parking" signs and increase enforcement. Sidewalks near schools should be curbed to reduce motorists' encroachment.



Motorists park along the sidewalk along the north side of Coquina Drive

Finding: The School District has provided a sidewalk that is separated from the vehicular access along Seminole Avenue. A student was observed bicycling through the nearby parking lot instead of using the sidewalk.

Recommendation: Students should continue to be encouraged to use the sidewalk connection to reduce possible conflict with motorists.



Campus sidewalk access is separated from vehicular traffic at the entrance from Seminole Avenue

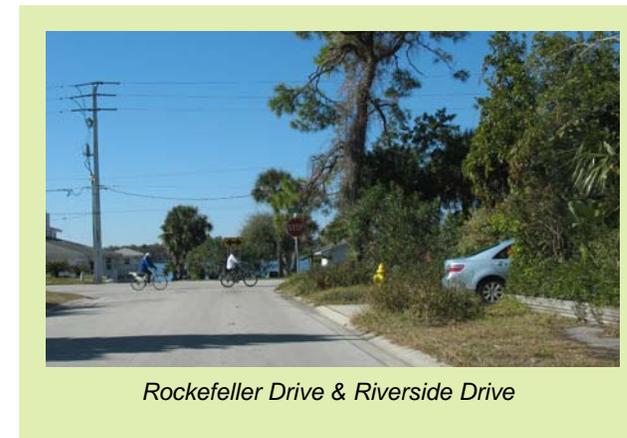
Findings: The intersection of Coquina Drive and Bosarvey Drive includes a one – directional stop sign along Coquina Drive. Motorists and sidewalk users may expect a standard 3-way stop at this location. Recent budget reductions resulted in the closing of a crossing guard location at this intersection. The car line for the school often extends to this intersection and increases the potential for conflict between motorists and pedestrians and bicyclists. Additionally, the crosswalk over Bosarvey Drive is faded.



Recommendations: A transportation engineer should review this intersection for the addition of a stop condition for southbound motorists at Coquina Drive. A standard 3-way stop may increase safety for motorists and sidewalk users at this intersection. The faded crosswalk over Bosarvey Drive should be repainted.

Finding: The sidewalk along the north side of Rockefeller Drive does not connect to the sidewalk along the east side of Riverside Drive.

Recommendation: Construct a minimum 5 foot wide section of sidewalk (50 linear feet) from the existing sidewalk on the north side of Rockefeller Drive to the existing sidewalk along the east side of Riverside Drive.



Finding: A drainage grate extends into Bosarvey Drive and may cause problems for bicyclists.

Recommendation: Replace the grate with an acceptable grate pattern to increase safety for bicyclists. As an interim measure and if feasible, turn the grate in the opposite direction.



Finding: Drainage grates are located in the curb ramps at the intersection of Carib Drive with Coquina Drive. The grates may pose problems for bicyclists, or people using strollers, skate boards or wheelchairs.

Recommendation: If possible, relocate drainage grates away from the sidewalk and ramp areas.



A drainage grate is located in a curb ramp at Carib Drive at Coquina Drive

Finding: At the time of this study, Halifax Drive was being resurfaced. Stop bars and crosswalks were missing along some intersections.

Recommendation: If not completed with construction, add stop bars and crosswalks to intersections along Halifax Drive and to any other intersections within the walk zone.



The crosswalk is missing over Seminole Avenue at Halifax Drive

Finding: Vegetation encroaches into the sidewalk along Coquina Drive and along other routes to school.

Recommendation: The City may wish to notify property owners of the encroachments and request removal. The City should work with the property owners to trim back vegetation.



Vegetation encroaches into the sidewalk along Coquina Drive



Cactus plants encroach into the sidewalk along the east side of Riverside Drive

Findings: The sidewalk along Halifax Drive is narrowed by vegetation encroachments in several areas. Vegetation overhangs the sidewalk in some areas.

Recommendations: Increase maintenance along this sidewalk where needed. Trim overhead vegetation to provide a clear walk zone. If possible, paint bicycle lanes or provide painted shoulders along Halifax Drive to increase the buffer to the sidewalk and provide an alternate route for adult bicyclists.



Vegetation and grass need to be trimmed along Halifax Drive to provide a clear walk zone

Finding: There are no signalized crossings over S.R. A1A at Osceola Avenue. A crosswalk over S.R. A1A on the north side of Osceola Avenue was removed with recent road improvements. There is a sidewalk on the south side of Osceola Avenue. Currently no students appear to live east of S.R. A1A.

Recommendations: Review the possibility of providing a signal at the intersection of S.R. A1A and Osceola Avenue, especially if students live on the east side of S.R. A1A. The crosswalk should align with the sidewalk along the south side of Osceola Avenue.



Sidewalk is located along the south side of Osceola Avenue



Pedestrians cross S.R. A1A at Osceola Avenue

Finding: The new curb ramp at the intersection of S.R. A1A and Osceola Avenue does not include an inside radius and sidewalk users, especially those in a wheelchair, may have difficulty maneuvering around the corner.

Recommendation: Reconstruct the sidewalk intersection to include an inside radius.



The new curb ramp at the intersection of S.R. A1A and Osceola Avenue does not include an inside radius

Finding: The painted crosswalk over Halifax Drive at Fluhart Drive is faded and may not be visible to motorists.

Recommendation: Re-paint the crosswalk.



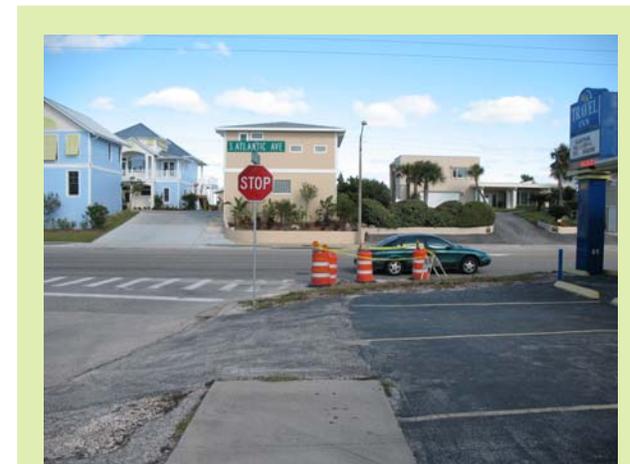
The crosswalk over Halifax Drive at Fluhart Drive is faded

Finding: Pull-in parking reduces safety for sidewalk users along the south side of Seminole Avenue and along Vining Court.

Recommendation: If possible, work with property owners to design alternative parking plans. Alternatively, provide paint markings to increase the visibility of the sidewalk and to increase awareness of the pedestrian zone.



Pull-in parking reduces sidewalk safety along Vining Court



Pull-in parking reduces sidewalk safety along Seminole Avenue near S.R. A1A

Finding: The majority of students living north of S.R. 40 live east of Halifax Drive. There are no sidewalks along the east side of Halifax Drive north of S.R. 40. To travel to school, these students would likely cross Halifax Drive unaided to reach the sidewalk on the west side of the road. The students would need to cross Halifax Road a second time at S.R. 40 (signalized) before crossing S.R. 40 (signalized).

Recommendation: Construct a minimum 5' wide sidewalk on the east side of Halifax Drive from S.R. 40 to Amsden Road. Sidewalk construction from S.R. 40 to Wildwood Avenue is the most needed. This project is recommended as a Priority Project and is further detailed at the end of this chapter.



The east right of way on Halifax Drive north of S.R. 40

Findings: The walk zone for Osceola Elementary school extends north of S.R. 40 (Granada Boulevard). The crossing guard position at S.R. 40 and Halifax Drive has been closed due to budget reductions and the lack of students using this crossing. Students living north of S.R. 40 must now cross S.R. 40 unaided. The intersection is signalized with pedestrian features. Two of the four pedestrian push button signals may not be accessible for all users.



Halifax Drive looking south across S.R. 40

Recommendations: A sidewalk on the east side of Halifax Drive north of S.R. 40 may result in more students walking or riding their bicycles to and from school. If this occurs, the need for a crossing guard at the location of Halifax Drive and S.R. 40 should be reviewed.

The push button signals on the southeast corner and the northwest corner of the intersection should be revised to comply with ADA.



The push button signal at the southeast intersection of S.R. 40 and Halifax Drive is not accessible to all users

Finding: The drop-off area for the Ormond Memorial Gardens includes curbed sidewalks without direct ramps along the east side of Halifax Drive that the sidewalk user may not expect.

Recommendations: The County may wish to work with the City of Ormond Beach to add curb ramps to allow through traffic on the sidewalk. As an interim measure, color the concrete surface within the drop-off area to distinguish the vehicular area from the sidewalk. Paint the curb yellow.



Curbing along the drop-off area of Ormond Memorial Gardens

Master Plan

The following Bicycle and Pedestrian Master Plan specifies a network of sidewalks and trails that, if implemented, will greatly increase the convenience and safety of travel for pedestrians and bicyclists. A 24"x36" fold-out Bicycle and Pedestrian Master Plan is included as a deliverable with this Study.

The Master Plan includes the following information:

Existing Conditions:

- Existing Walk Zone area from School District GIS (shaded tan)
- Student Home Locations from School District (dark blue dot)
- Existing Trails from County GIS and field review (solid green line)
- Existing Wide Sidewalk 6-8' from field review (solid blue line)
- Existing Sidewalks 3-5' from field review (solid red line)
- Existing Crossing Guard Locations from County GIS and field observations (green cross for existing to remain and yellow cross for recently closed)
- Existing Votran Routes and stops from Volusia County MPO (yellow lines along roadway centerline with yellow squares at bus stops).

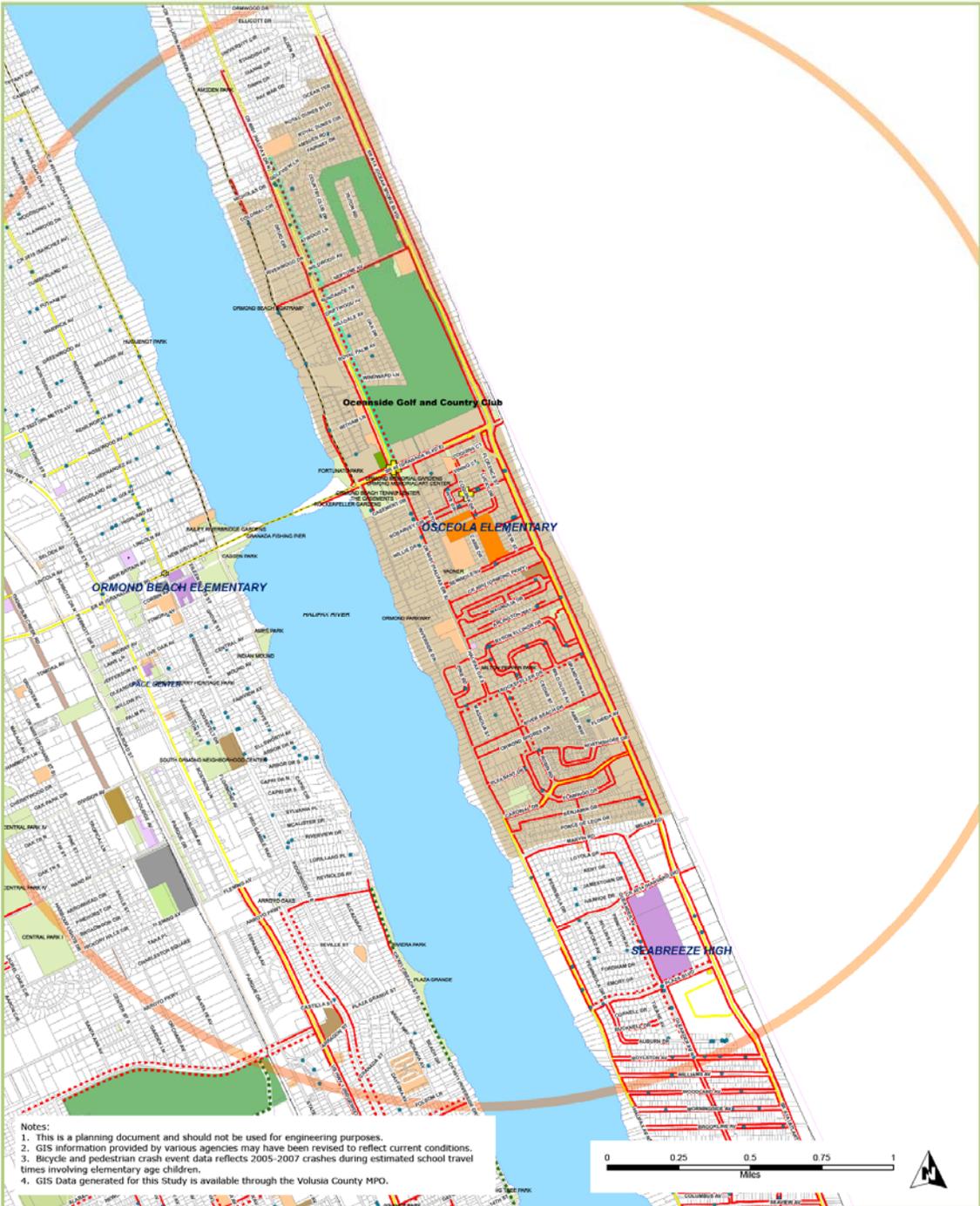
Planned Conditions:

- Planned trail (long dashed dark green line)
- Planned wide sidewalk (long dashed blue line)
- Planned sidewalk (long dashed red line)

Recommendations of this Study:

- Proposed Trails (dashed dark green line)
- Proposed Sidewalks 6-8' wide (dashed blue line)
- Proposed Sidewalks 5' (dashed red line)

This Master Plan and supplemental information supplied within the Implementation Report are meant as a guideline for sidewalk and trail planning. The Master Plan is not an engineering document.



Notes:
 1. This is a planning document and should not be used for engineering purposes.
 2. GIS information provided by various agencies may have been revised to reflect current conditions.
 3. Bicycle and pedestrian crash event data reflects 2005-2007 crashes during estimated school travel times involving elementary age children.
 4. GIS Data generated for this Study is available through the Volusia County MPO.

<p>Trails/Sidewalks</p> <ul style="list-style-type: none"> — Existing Sidewalk - - - Planned Sidewalk · · · Proposed Sidewalk — Existing Wide Sidewalk - - - Planned Wide Sidewalk · · · Proposed Wide Sidewalk — Existing Trail - - - Planned Trail · · · Proposed Trail — Existing Unpaved Trail — MPO Bicycle Route 	<p>Location Information</p> <ul style="list-style-type: none"> ● Student Home Location ⚡ Recently Closed Crossing Guard Location — Existing Transit Route (weekday) <p>Recommended Priority Project:</p> <ul style="list-style-type: none"> — Halifax Drive Sidewalk (east side) 	<p>Walk Zone Information</p> <ul style="list-style-type: none"> — School Walk Zone ○ 2 - Mile Radius 	<p>Property Identification</p> <ul style="list-style-type: none"> — School Studied — School Property - Public — School Property - Private — Utility Owned Property — Church Owned Property — Publicly Owned Property — Park/ Conservation/ Golf — City Jurisdiction
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	<p>Osceola Elementary School Bicycle and Pedestrian Master Plan Ormond Beach, Florida</p>	<p>FUNDING PROVIDED BY THE FLORIDA DEPARTMENT OF TRANSPORTATION DISTRICT FIVE</p> <p>June 2009</p>
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Recommended Priority Project

Halifax Drive Sidewalk (east side – north of S.R. 40)

School Served: Osceola Elementary School

Project Location: East side of Halifax Drive from Granada Boulevard to Amsden Road

Project Description: A minimum 5 foot wide concrete sidewalk, 1.15 miles long.

Findings: Most students living north of S.R. 40 live east of Halifax Drive. These students most likely cross Halifax Drive at various locations to reach the sidewalk on the west side. The students must then cross Halifax Drive again at S.R. 40 to reach the east side of the road to access the school.

Recommendations: Construct a minimum 5' wide sidewalk along the east side of Halifax Drive from Amsden Road to S.R. 40. If this is not feasible, review the possibility of adding wide shoulders or bicycle lanes to both sides of Halifax Drive.

Maintaining Agency: City of Ormond Beach

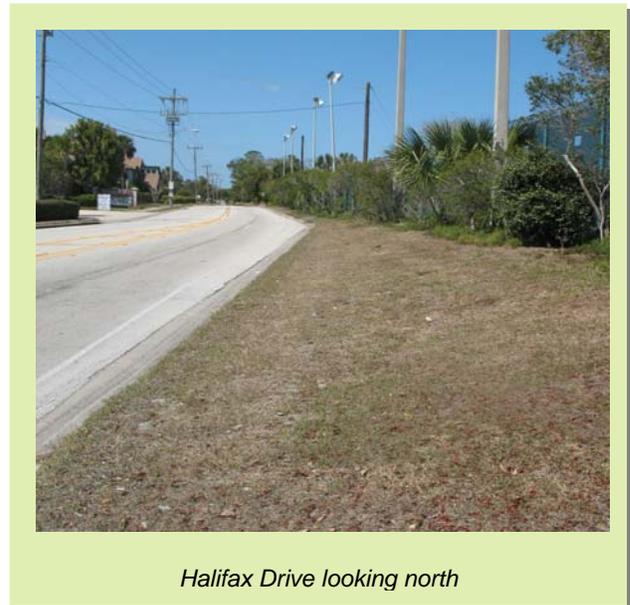
Potential Constraints: Limited right of way and grading challenges may increase the difficulty and cost to construct this sidewalk. Vegetation removal will be necessary in some areas. A fairly low number of students would benefit from this sidewalk.

Potential Opportunities: More students may choose to walk or ride their bicycles to school if they could travel on a sidewalk on the east side of Halifax Drive. Students living east of Halifax Drive and north of S.R. 40 would no longer need to cross Halifax Drive two times to travel to and from school.

The City of Ormond Beach may have access to project funding through Community Development Block Grant or Tax Increment Financing programs.

Estimated Project Cost (not including right of way costs, if applicable): \$755,000

Note: The Priority Project is highlighted in blue on the corresponding Bicycle and Pedestrian Master Plan.



Right of way is particularly restricted along the Oceanside Country Club and along the parking lot near S.R. 40 (see photographs below).

